

Addendum No. 1

TRAFFIC SIGNAL & ADA IMPROVEMENTS

**TOWN OF ADDISON, TEXAS
PUBLIC WORKS AND ENGINEERING SERVICES
BID NUMBER 22-133**

06/16/2022



To: All Plan Holders of Record

This addendum forms part of the Contract Documents and modifies the Contract Documents and Specifications as noted below. Acknowledge receipt of the Addendum in the space provided on the Proposal Form and on the outer envelope of the Bid Proposal. Failure to acknowledge receipt of this Addendum may subject the Bidder to disqualification.

This addendum consists of 1 page and 2 attachments.

CHANGES TO SPECIFICATIONS

1. Section TS – Technical Specifications

Insert the attached pages TS-3, L1.14, and L1.11.

CHANGES TO PLANS

1. REPLACE PLAN SHEETS 5, 11, 17, 43, and 43 with the plan sheets attached. Revisions were made to show curbed areas of pedestrian ramps and adjust sidewalk for pedestrian pole clearances and ramp landings. Revisions have been noted on each sheet.

END OF ADDENDUM NO. 1

Landscape Paver Specifications

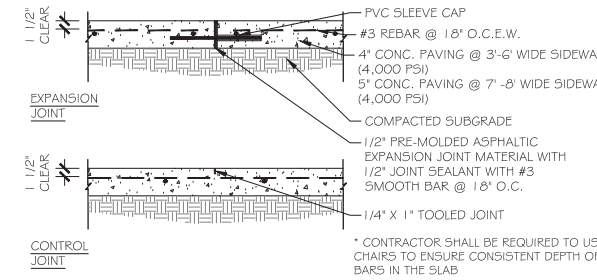
Landscape pavers are proposed for installation at Belt Line Road & Addison Road and Belt Line Road & Beltway Drive. Per Town of Addison, landscape paver type, color, and pattern shall match the specifications from the Belt Line 1.5 project.

See detail 4 on the following page (L1.14) for paver specifications and sheet L1.11 for an example layout. The photos below also illustrate the paver type, color, and pattern.



- NOTE:
1. COMPACTED SUBGRADE - COMPACT TO 95% STANDARD PROCTOR DENSITY AT 0 TO +/- 2% OPTIMUM MOISTURE CONTENT AS PER ASTM D 698 (TYP.)
 2. EXPANSION JOINTS & CONTROL JOINTS SHALL BE LOCATED ACCORDING TO THE FOLLOWING:
 - EJ: CHANGES IN DIRECTION, MAX 75' O.C., AGAINST CURBS, BUILDINGS, AND MANHOLES.
 - CJ: SPACED EVENLY TO PRODUCE SQUARE MODULES UP TO 8' O.C. MAX

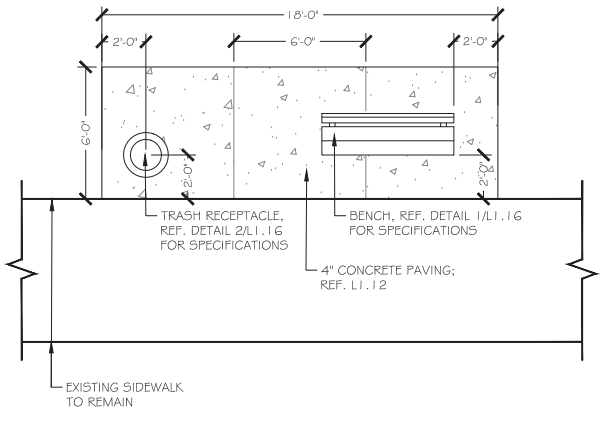
- CONCRETE MIXTURE:
1. COMPRESSIVE STRENGTH 28 DAYS, 4,000 PSI.
 2. MAXIMUM WATER-CEMENTITIOUS MATERIALS RATIO AT POINT OF PLACEMENT 0.45.
 3. SLUMP LIMIT: 5 INCHES PLUS OR MINUS 1 INCH
 4. AIR CONTENT: 5-1/2 PERCENT PLUS OR MINUS 1.5 PERCENT
 5. MEASURE, BATCH, AND MIX CONCRETE MATERIALS AND CONCRETE ACCORDING TO ASTM C 94/C 94M.
 6. NATURAL GREY, MEDIUM BROOM FINISH, PROVIDE SAMPLES PER SPEC



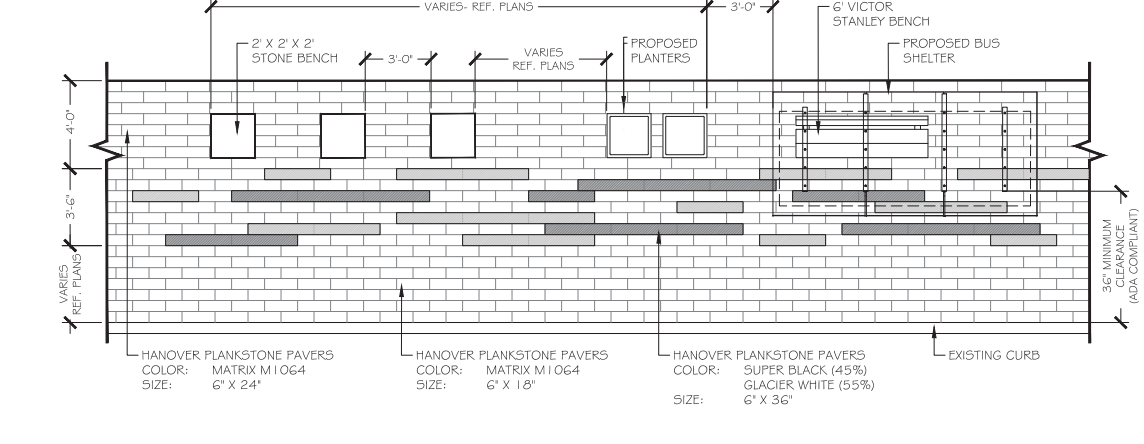
* CONTRACTOR SHALL BE REQUIRED TO USE CHAIRS TO ENSURE CONSISTENT DEPTH OF BARS IN THE SLAB

REDWOOD EXPANSION JOINTS NOT ALLOWED

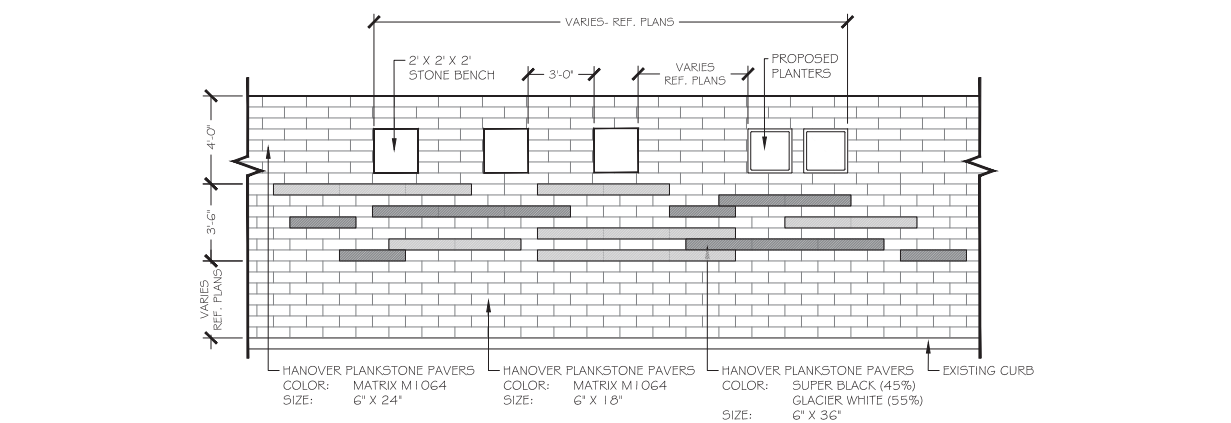
1 CONCRETE PAVING SECTION
3/4" = 1'-0"



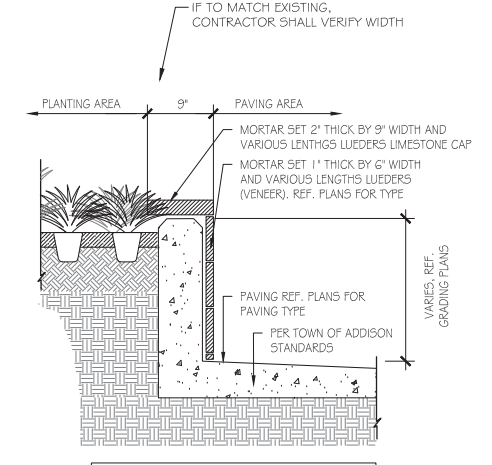
2 BUS STOP LAYOUT
1/4" = 1'-0"



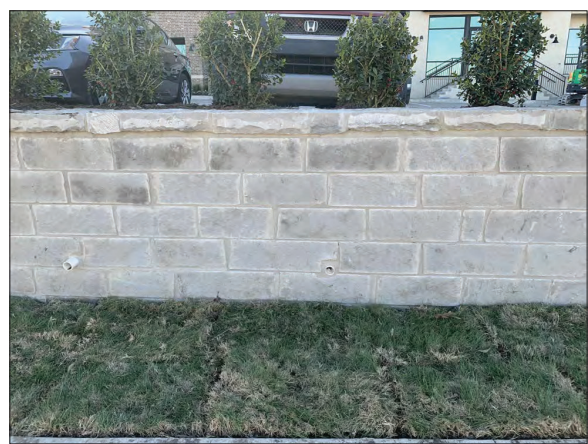
3 STANDARD PAVER LAYOUT AT BUS SHELTER
1/4" = 1'-0"



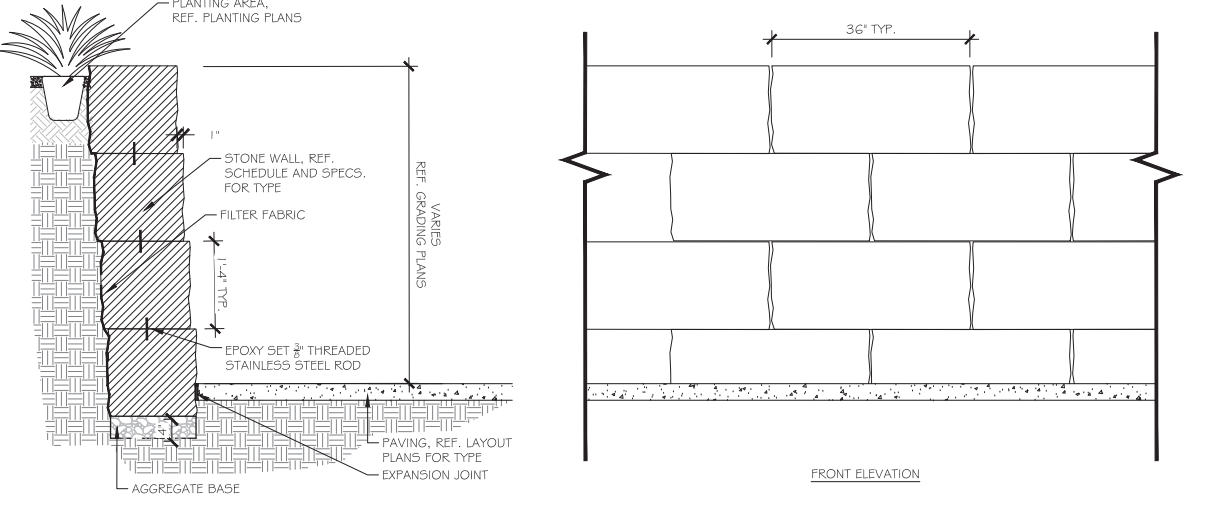
4 STANDARD PAVER LAYOUT AT INTERSECTIONS
1/4" = 1'-0"



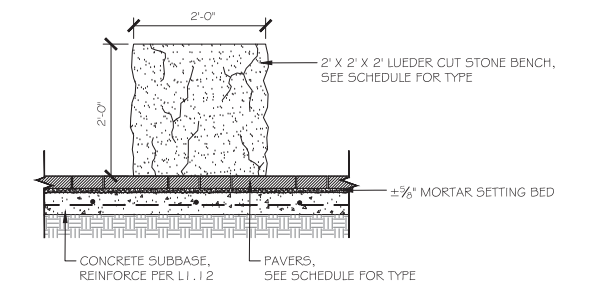
5 STONE VENEER SITE WALL
1" = 1'-0"



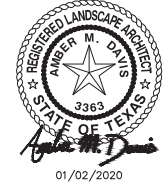
EXISTING STONE VENEER WALL AT ADDISON GROVES LOCATION; PROPOSED VENEER TO MATCH; REF. PLANS FOR STONE TYPE



6 LOW STONE BLOCK RETAINING WALL
3/4" = 1'-0"



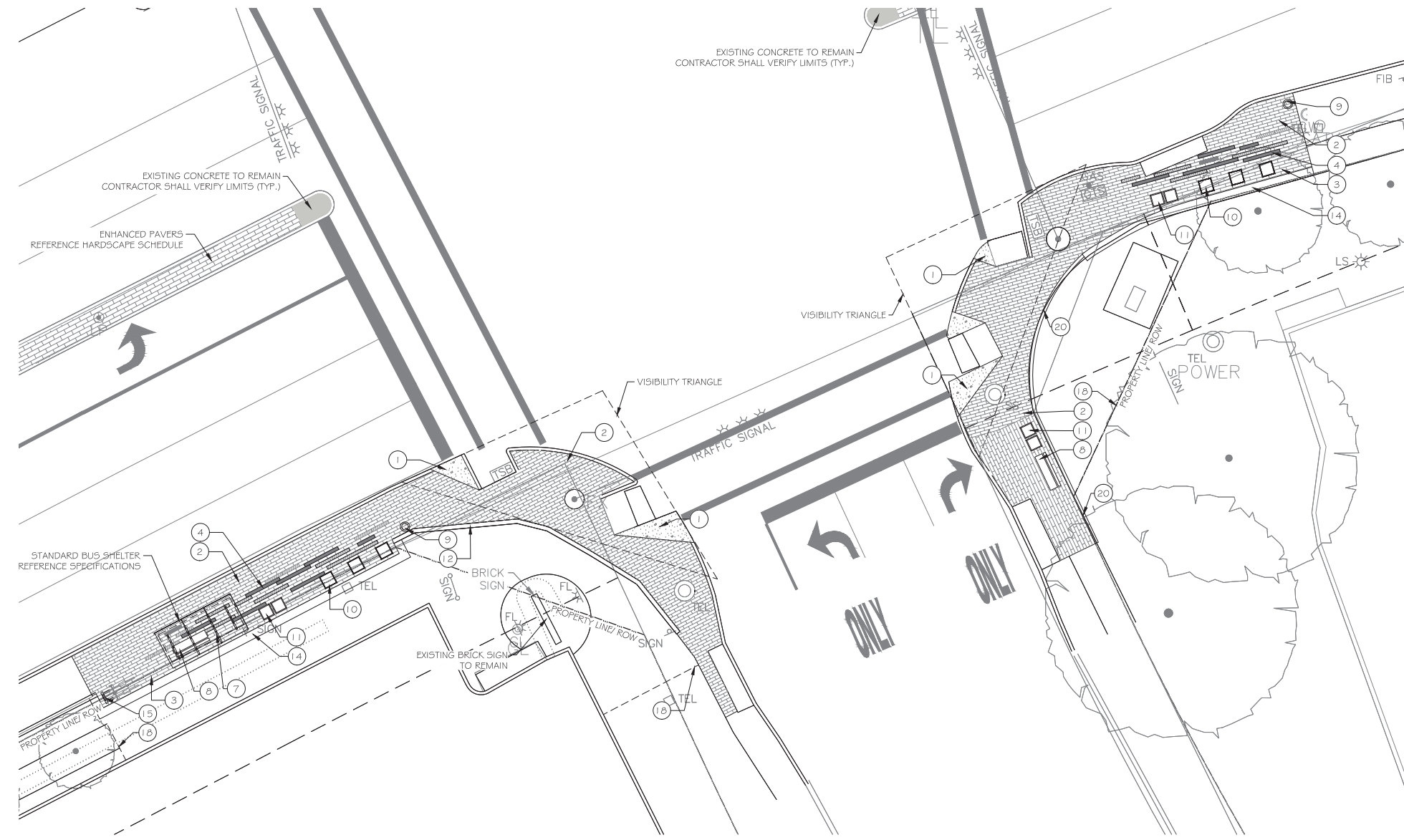
7 LUEDERS CUT STONE BENCH
3/4" = 1'-0"



NO.	DATE	ISSUE
1	01/02/2020	100% CONSTRUCTION DOCUMENTS
2	12/09/2019	90% CONSTRUCTION DOCUMENTS
3	07/30/2019	DESIGN DEVELOPMENT
4	06/18/2019	SCHEMATIC DESIGN

Pacheco Koch
4080 BRYANT IRVIN ROAD
FORT WORTH, TX 76109 817.412.7155
TX REG. ENGINEERING FIRM F-469
TX REG. SURVEYING FIRM LS-10008001

HARDSCAPE DETAILS						
BELT LINE 1.5						
LANDSCAPE ENHANCEMENTS						
CONSTRUCTION DOCUMENTS						
TOWN OF ADDISON, DALLAS COUNTY, TEXAS						
DESIGN	DRAWN	DATE	SCALE	NOTES	FILE	NO.
AMD	AMW	JAN 2020	AS SHOWN			L1.14



REFERENCE NOTES SCHEDULE

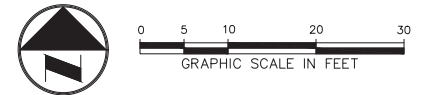
SYMBOL	DESCRIPTION
1	STANDARD PEDESTRIAN CONCRETE PAVING
2	ENHANCED PEDESTRIAN PAVERS W/ EXISTING CONCRETE BASE BY HANOVER OR APPROVED EQUAL TYPE: PLANKSTONE PAVERS SIZE: 6' X 18", THICKNESS 2 1/2" COLOR: MATRIX M1064 CONTACT: DIANE COLLIER, AIA 214.238.3100 WWW.COLLIERGROUPTX.COM
2A	ENHANCED PEDESTRIAN PAVERS W/ NEW CONCRETE BASE
3	ENHANCED PEDESTRIAN PAVERS BY HANOVER OR APPROVED EQUAL TYPE: PLANKSTONE PAVERS SIZE: 6' X 24", THICKNESS 2 1/2" COLOR: MATRIX M1064 CONTACT: DIANE COLLIER, AIA 214.238.3100 WWW.COLLIERGROUPTX.COM
4	ENHANCED PEDESTRIAN PAVERS BY HANOVER OR APPROVED EQUAL TYPE: PLANKSTONE PAVERS SIZE: 6' X 36", THICKNESS 2 1/2" COLOR: SUPER BLACK (55% GLACIER WHITE (45% CONTACT: DIANE COLLIER, AIA 214.238.3100 WWW.COLLIERGROUPTX.COM
5	ENHANCED PEDESTRIAN PAVERS W/ EXISTING CONCRETE BASE BY PAVESTONE, SIZE 4'X8", THICKNESS, 80MM TYPE: KEYSTONE VERONA, QUARTEX FINISH COLOR: CHARCOAL CONTACT: JOEY GUEDEA, JOEYG@KEYSTONEHARDSCAPES.COM WWW.KEYSTONEHARDSCAPES.COM
5A	ENHANCED PEDESTRIAN PAVERS W/ NEW CONCRETE BASE
6	TEKWAY HIGH PERFORMANCE ADA DOME-TILE MANUFACTURER: STRONGGO INDUSTRIES THICKNESS: 3/4" COLOR: TO BE DETERMINED BY OWNER
7	BUS SHELTER WITH SOLAR LIGHTING GLASS BY: LANDSCAPE FORMS TYPE: CONNECT 2.0 TOWN OF ADDISON STANDARD SIZE: 12'X6" CONTACT: LARA MOFFAT (800.430.6206 X 1309)
8	6" BENCH BY VICTOR STANLEY OR APPROVED EQUAL ITEM RB-28 COLOR: BLACK
9	36 GALLON TRASH RECEPTACLE BY VICTOR STANLEY OR APPROVED EQUAL ITEM S-42 COLOR: BLACK

CONTINUED

10	2' X 2' X 2' LUEDERS CUT STONE BENCH SAWCUT TOP AND BOTTOM ONLY ROUGHBACK FINISH ON FRONT, BACK AND ENDS COLOR: CHARCOAL (LIGHT) BY: VAQUERO STONE AND SUPPLY CONTACT: JEFF WILSON (972) 241-1575
11	2' X 2' X 2' STEEL PLANTER BY FORM AND FIBER OR APPROVED EQUAL TYPE: BOX SERIES (STEEL) COLOR: ADDISON BLUE CONTACT: CHRISTINA EVANS, 870.330.1510 LIGHTSPEK.COM
12	STONE VENEER (LEUDER) AT EXISTING RETAINING WALL STONE SIZE: 6'X12", RUNNING BOND BY: VAQUERO STONE AND SUPPLY CONTACT: JEFF WILSON (972) 241-1575
13	RETAINING WALL W/ STONE VENEER (LEUDER) STONE SIZE: 6'X12", RUNNING BOND BY: VAQUERO STONE AND SUPPLY CONTACT: JEFF WILSON (972) 241-1575
14	LOW CUT STONE RETAINING WALL SAWCUT ON 4 SIDES ROUGHBACK FINISH ON FRONT AND BACK COLOR: (CHARCOAL, LIGHT) BY: VAQUERO STONE AND SUPPLY CONTACT: JEFF WILSON (972) 241-1575
15	BIKE RACK BY VICTOR STANLEY OR APPROVED EQUAL ITEM BRWS-101 COLOR: BLACK
16	GATEWAY MONUMENT
17	DIRECTIONAL RAMP TOWN OF ADDISON STANDARD RAMP
18	STEEL EDGING 3/16" THICKNESS
19	LEANING RAILS BY: LANDSCAPE FORMS TYPE: CONNECT RAIL- LEANING RAIL SIZE: 6'X45'X30" CONTACT: LARA MOFFAT (800.430.6206 X 1309)
20	6" CONCRETE PAVER EDGE RESTRAINT

CONTINUED

1 ENLARGEMENT B2 (SURVEYOR)
1" = 10'-0"



NO.	DATE	ISSUE
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3	07/30/2019	DESIGN DEVELOPMENT
4	06/18/2019	SCHEMATIC DESIGN

NO. DATE REVISION
Pacheco Koch
 4080 BRYANT IRVIN ROAD
 FORT WORTH, TX 76109 817.412.7155
 TX REG. ENGINEERING FIRM F-469
 TX REG. SURVEYING FIRM LS-10008001

DESIGN	DRAWN	DATE	SCALE	NOTES	FILE	NO.
AMD	AMW	JAN 2020	AS SHOWN			L1.11

BELT LINE 1.5 LANDSCAPE ENHANCEMENTS

ITEM 416, DRILL SHAFTS:

PROVIDE A FORMED SMOOTH FINISH FOR ALL PORTIONS OF DRILL SHAFTS EXTENDING ABOVE PROPOSED GROUND. INCLUDE COST FOR THIS WORK IN THE UNIT BID PRICE FOR THIS ITEM.

TRAFFIC SIGNAL POLE FOUNDATIONS WILL BE PAID FOR ONCE REGARDLESS OF EXTRA WORK CAUSED BY OBSTRUCTIONS.

INSTALL A 5/8"x10' COPPER CLAD GROUND ROD IN EACH TRAFFIC SIGNAL POLE FOUNDATION. THE GROUND ROD FOR EACH FOUNDATION WILL PROTRUDE ABOVE THE FINISH GRADE OF THE FOUNDATION A MINIMUM OF 1" AND A MAXIMUM OF 2".

CONCRETE REMOVAL REQUIRED FOR INSTALLATION OF DRILLED SHAFTS WILL BE SUBSIDIARY TO ITEM 416.

FORM A 3/4-INCH CHAMFER ON THE TOP EDGE OF EACH SIGNAL POLE FOUNDATION.

PROBE FOR UTILITIES AND UNDERGROUND STRUCTURES PRIOR TO DRILLING FOUNDATIONS. FOUNDATIONS SHALL BE PAID FOR ONCE REGARDLESS OF EXTRA WORK CAUSED BY OBSTRUCTIONS.

ITEM 528, LANDSCAPE PAVERS:

PER TOWN OF ADDISON, LANDSCAPE PAVERS SHALL BE HANOVER PLANKSTONE PAVERS. SEE PROJECT MANUAL FOR ADDITIONAL DETAILS ON PAVES COLOR AND DIMENSIONS.

ITEM 531, CURB RAMPS:

PER TOWN OF ADDISON, CHARCOAL GREY DETECTABLE WARNING PLATES SHALL BE USED.

ITEM 618, CONDUITS:

THE LOCATION OF CONDUITS AND GROUND BOXES ARE DIAGRAMMATIC ONLY AND MAY BE SHIFTED TO ACCOMMODATE FIELD CONDITIONS AS DIRECTED.

SECURE PERMISSION AND APPROVAL FROM THE PROPER AUTHORITY PRIOR TO CUTTING INTO OR REMOVING ANY SIDEWALKS OR CURBS FOR INSTALLATION OF THIS ITEM.

WHEN HOLES ARE DRILLED THROUGH CONCRETE STRUCTURES, USE A CORING DEVICE. DO NOT USE MASONRY OR CONCRETE DRILLS.

PLACE CONDUIT UNDER EXISTING PAVEMENT BY AN APPROVED BORING METHOD. DO NOT PLACE BORING PITS CLOSER THAN 2 FEET FROM THE EDGE OF THE PAVEMENT UNLESS OTHERWISE DIRECTED. DO NOT USE WATER JETTING. WHEN CONDUITS ARE BORED, DO NOT EXCEED 18 INCHES IN THE VERTICAL AND HORIZONTAL TOLERANCES AS MEASURED FROM THE INTENDED TARGET POINT.

DO NOT USE A PNEUMATICALLY DRIVEN DEVICE FOR PUNCHING HOLES BENEATH THE PAVEMENT (COMMONLY KNOWN AS A "MISSILE").

FURNISH AND INSTALL A NON-METALLIC PULL ROPE IN CONDUIT RUNS IN EXCESS OF 50 FEET.

USE A COLORED CLEANER-PRIMER ON ALL PVC TO PVC JOINTS BEFORE APPLICATION OF PVC CEMENT.

SEAL ALL CONDUIT ENDS WITH A PERMANENTLY SOFT, NON-TOXIC DUCT SEAL. USE A DUCT SEAL THAT DOES NOT ADVERSELY AFFECT OTHER PLASTIC MATERIALS OR CORRODE METALS.

FURNISH AND INSTALL NON-METALLIC PULL ROPES IN CONDUIT INSTALLED FOR FUTURE USE AND CAP USING STANDARD WEATHER-TIGHT CONDUIT CAPS, AS APPROVED. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT IS SUBSIDIARY TO THIS ITEM.

WHEN USING EXISTING CONDUIT, ENSURE THAT ALL CONDUITS HAVE BUSHINGS AND ARE CLEANED OF MUD AND DEBRIS. RE-STRAP CONDUIT THAT IS BEING RELOCATED TO NEW TIMBER POLES AS IF IT WERE A NEW INSTALLATION. THIS WORK WILL NOT BE PAID FOR DIRECTLY, BUT IS SUBSIDIARY TO THIS ITEM.

ITEM 620, CONDUCTORS:

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE IDENTIFIED BY A CONTINUOUS GREEN COLORED JACKET INSULATION OR BARE WIRE. GROUNDED CONDUCTORS (NEUTRAL) SHALL BE IDENTIFIED BY A CONTINUOUS WHITE COLORED JACKET. UNGROUNDED CONDUCTORS (HOT) IN A 120/240V OR 240/480V SYSTEM SHALL BE IDENTIFIED BY EACH POLE OR LEG. FOR 240-VOLT BRANCH CIRCUIT FED FROM 120/240 SOURCE AND 480-VOLT BRANCH CIRCUIT FED FROM 240/480 SOURCE, ENSURE ONE LEG IS IDENTIFIED BY A CONTINUOUS BLACK COLORED JACKET AND THE OTHER LEG BY A CONTINUOUS RED COLORED JACKET.

FOR PED POLES (ITEM 687) WITHIN THE PROJECT, PROVIDE SINGLE-POLE BREAKAWAY DISCONNECTS. USE BUSSMAN HEBW, LITTLEFUSE LEB, FERRAZ-SHAMMUT FEB, OR EQUAL ON UNGROUNDED CONDUCTORS. FOR ALL GROUNDED CONDUCTORS USE BUSSMAN HET, LITTLEFUSE LET, FERRAZ-SHAMMUT FEBN, OR EQUAL. THESE BREAKAWAY CONNECTORS HAVE A WHITE COLORED MARKING AND A PERMANENTLY INSTALLED SOLID NEUTRAL.

ITEM 624, GROUND BOXES:

SLACK CONDUCTORS REQUIRED BY STANDARD SHEET ED(2)-03 WILL BE SUBSIDIARY TO ITEM 624.

CONCRETE REMOVAL REQUIRED FOR INSTALLATION OF GROUND BOXES WILL BE SUBSIDIARY TO ITEM 624.

ITEM 680, INSTALLATION OF TRAFFIC SIGNALS:

REQUIREMENTS FOR THIS ITEM INCLUDE THE FOLLOWING WORK, ALL OF WHICH ARE SUBSIDIARY TO THIS ITEM:

1. NOTIFY THE TOWN OF ADDISON ONE WEEK BEFORE BEGINNING ANY WORK INVOLVING TRAFFIC SIGNALS.
2. PROVIDE SUBMITTAL LITERATURE FOR ALL TRAFFIC SIGNAL EQUIPMENT FOR TOWN APPROVAL BEFORE INSTALLATION.
3. PROVIDE DETECTOR CARDS. PROVIDE A USER'S MANUAL WITH FULL OPERATING INSTRUCTIONS AND THE CONTACT NAME, ADDRESS, AND TELEPHONE NUMBER FOR THE REPRESENTATIVE, MANUFACTURER, OR DISTRIBUTOR FOR WARRANTY REPAIR.
4. THE FOLLOWING ITEMS WILL BE FURNISHED AND INSTALLED BY THE CONTRACTOR: SIGNAL CABINET, ALL INTERNAL HARDWARE, AND VIVDS EQUIPMENT. SIGNAL CABINET SHALL BE NEMA TS-2. SIGNAL CONTROLLER SHALL BE ECONOLITE COBALT, AND MMU SHALL BE EDI MMU2-16LEIP.
5. RELOCATE BATTERY BACKUP UNIT FROM EXISTING CABINETS.
6. CONNECT ALL FIELD WIRING TO THE CONTROLLER ASSEMBLY.
7. FURNISH AND INSTALL ALL SIGN PANELS FOR MOUNTING ON SIGNAL POLES AND MAST ARMS. FABRICATE THE SIGN PANELS IN ACCORDANCE WITH ITEM 636, AND MOUNT WITH ASTRO-SIGN BRAC, SIGNFIX ALUMINUM CHANNEL, OR EQUAL AS APPROVED BY THE ENGINEER.
8. FURNISH AND INSTALL ALL OTHER SIGNS IN ACCORDANCE TO ITEM 636. FURNISH ALL MOUNTING HARDWARE FOR ALL SIGNS. MOUNT SIGNS WITH ASTRO-SIGN BRAC, SIGNFIX ALUMINUM CHANNEL, OR EQUAL AS APPROVED BY THE ENGINEER.
9. RELOCATE ILSNS FROM EXISTING MAST ARMS.
10. INSTALL THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT.
11. HAVE A QUALIFIED TECHNICIAN ON THE PROJECT SITE TO PLACE THE TRAFFIC SIGNAL IN OPERATION.
12. USE QUALIFIED PERSONNEL TO RESPOND TO AND DIAGNOSE ALL TROUBLE CALLS DURING THE THIRTY-DAY TEST PERIOD. REPAIR ANY MALFUNCTION TO CONTRACTOR-SUPPLIED SIGNAL EQUIPMENT. PROVIDE TO THE ENGINEER A LOCAL TELEPHONE NUMBER, NOT SUBJECT TO FREQUENT CHANGES AND AVAILABLE ON A 24-HOUR BASIS, FOR REPORTING TROUBLE CALLS. RESPONSE TIME TO REPORTED CALLS MUST BE LESS THAN 2 HOURS. MAKE APPROPRIATE REPAIRS WITHIN 24 HOURS. PLACE A LOGBOOK IN THE CONTROLLER CABINET AND KEEP A RECORD OF EACH TROUBLE CALL REPORTED. NOTIFY THE ENGINEER OF EACH TROUBLE CALL. DO NOT CLEAR THE ERROR LOG IN THE CONFLICT MONITOR OR MMU DURING THE THIRTY-DAY TEST PERIOD WITHOUT APPROVAL.
13. PREVENT ANY DAMAGE TO PROPERTY OWNERS' S POLES, FENCES, SHRUBS, ETC. PROTECT ALL UNDERGROUND AND OVERHEAD UTILITIES AND REPAIR ANY DAMAGE. PROVIDE ACCESS TO ALL DRIVEWAYS DURING CONSTRUCTION.

ITEM 682, SIGNAL HEADS:

INSTALL SIGNAL HEAD ATTACHMENTS SO THAT THE WIRING TO EACH SIGNAL HEAD PASSES FROM THE MAST ARM THROUGH THE ATTACHMENT HARDWARE TO THE SIGNAL HEAD. DO NOT LEAVE CABLE OR WIRING EXPOSED.

PROVIDE SIGNAL HEAD ATTACHMENTS THAT ALLOW FOR ADJUSTMENT ABOUT THE HORIZONTAL AND VERTICAL AXIS.

TURN DOWN SIGNAL HEADS OR COVER WITH BURLAP OR OTHER MATERIAL, AS APPROVED, UNTIL TRAFFIC SIGNAL IS PLACED IN OPERATION.

MOUNT SIGNAL HEADS LEVEL AND PLUMB AND AIMED AS DIRECTED. MATCH EXISTING SIGNAL HEAD SECTION COLOR (BLACK).

ITEM 684, SIGNAL CABLE:

PROVIDE STRANDED 14 AWG TYPE A SIGNAL CABLES.

PROVIDE A SEPARATE MULTI-CONDUCTOR SIGNAL CABLE (14 AWG) INSIDE PEDESTAL POLES AND SIGNAL POLES FROM THE TERMINAL STRIP TO EACH SIGNAL HEAD AS SHOWN ON THE PLANS.

IDENTIFY EACH CABLE AS SHOWN ON THE PLANS (CABLE 1, ETC.) WITH PERMANENT MARKING LABELS (PANDUIT TYPE PLM STANDARD SINGLE MARKER TIE, THOMAS&BETTS TYPE 548M, OR EQUAL) AT EACH GROUND BOX, POLE BASE, AND CONTROLLER.

ITEM 686, SIGNAL POLES:

ALL MAST ARM POLE ASSEMBLIES, AS SUPPLIED AND INSTALLED, MUST CONFORM TO THE DETAILED DRAWINGS AND/OR REQUIREMENTS IN THE PLANS AS TO HEIGHT, GENERAL DESIGN, AND FINISH. THE CONTRACTOR SHALL SUBMIT FOR APPROVAL BY THE ENGINEER, FIVE (5) PRINTS OF DRAWINGS WHICH SHOW ALL PERTINENT INFORMATION AND DATA REQUIRED FOR VERIFYING STRUCTURAL ADEQUACY, AND ALL FABRICATION AND ERECTION DETAILS.

PROVIDE 12 CIRCUIT BUCHANAN TYPE 112SN, KULKA TYPE 985-GP-12 CU, OR EQUAL TERMINAL STRIPS IN THE SIGNAL POLE ACCESS COMPARTMENT. PROVIDE ADDITIONAL TERMINAL STRIPS OF 8 CIRCUITS EACH WHEN MORE THAN 12 CIRCUITS ARE REQUIRED. THE CONDUCTORS FOR THE LINE AND LOAD SIDE OF THE TERMINAL STRIP SHALL BE IDENTIFIED WITH A PLASTIC LABEL WITH TWO STRAPS PER TAG. THE LINE SIDE SHALL HAVE EACH SIGNAL HEAD, PED HEAD, AND PUSH BUTTON IDENTIFIED ON THE TAG.

MARK POLE SHAFTS AND MAST ARMS WITH THE IDENTIFICATION NUMBERS FROM THE PLANS TO FACILITATE FIELD-ASSEMBLY. IDENTIFY POLE SHAFTS AND MAST ARMS BY INTERSECTION FOR PROJECTS WITH MULTIPLE INTERSECTIONS.

PROVIDE NUTS ON TOP AND BOTTOM (DOUBLE NUTS) OF THE BASE PLATE AS SHOWN ON THE PLANS.

SET ANCHOR BOLTS FOR MAST ARM SIGNAL POLES AND STRAIN POLES SO THAT TWO ARE IN TENSION AND TWO ARE IN COMPRESSION. OBTAIN APPROVAL OF ANCHOR BOLT PLACEMENT BEFORE PLACING CONCRETE.

USE THE TRAFFIC SIGNAL POLE HEIGHTS AND MAST ARM LENGTHS SHOWN ON THE PLANS AND IN THE MATERIAL SUMMARY FOR BIDDING PURPOSES ONLY. MAKE FIELD MEASUREMENTS TO DETERMINE THE ACTUAL POLE HEIGHT AND MAST ARM LENGTH REQUIRED. PROVIDE VERTICAL CLEARANCE OF 17 TO 19 FEET FROM THE ROADWAY TO THE LOWEST POINT OF THE SIGNAL HEAD OR MAST ARM. PLACE SIGNAL HEADS 40 FEET MINIMUM AND 180 FEET MAXIMUM FROM THE STOP LINE. IF THE NEAREST SIGNAL IS MORE THAN 180 FEET FROM THE STOP LINE, PLACE A SUPPLEMENTAL NEAR-SIDE SIGNAL HEAD. DETERMINE THE FIELD MEASUREMENTS AND ELEVATIONS FROM THE ACTUAL FIELD LOCATION OF THE POLES, CONSIDERING ALL ABOVE AND BELOW GROUND UTILITIES AND EXISTING ROADWAY ELEVATIONS.

ITEM 686, SIGNAL POLES (CONTINUED):

PROVIDE VIBRATION DAMPERS FOR MAST ARMS 28 FEET TO 48 FEET IN LENGTH. INSTALL AS SHOWN ON MA-DPD-12.

FOR EXISTING SIGNAL POLES, REPLACEMENT OF EXISTING CONDUCTORS IS NOT REQUIRED INSIDE THE POLES. PLUG ANY UNUSED OPENINGS IN EXISTING MAST ARMS AND POLES WITH AN APPROVED MATERIAL.

PROVIDE 3 PIPE PLUGS FOR WIRING ACCESS ON STRAIN POLES.

PROVIDE A THREE PIECE BRACKET ASSEMBLY ON STRAIN POLES OR DRILL THE POLE AND USE THIMBLE EYE BOLTS TO ATTACH THE STRAIN VISE FOR THE SPAN WIRE.

PER TOWN OF ADDISON, SIGNAL POLE AND MAST ARMS AT BELT LINE ROAD & BELTWAY DRIVE AND BELT LINE ROAD & ADDISON ROAD TO BE POWDER COATED FROM IFS COATINGS, INC. PRODUCT # SRSL 90259, BATCH # C11341, DESCRIPTION: KIM PLATINUM SILVER.

ITEM 688, PUSH BUTTONS:

PROVIDE PEDESTRIAN PUSH BUTTON ASSEMBLIES THAT HAVE PERMANENT-TYPE SIGNS WITHIN THE DETECTOR UNIT WHICH INDICATES WHICH CROSSWALK SIGNAL IS ACTUATED. PROVIDE PUSH BUTTONS WITH A MINIMUM 2 INCH CONVEX PLUNGER. PROVIDE A PROTECTIVE SHROUD ENCIRCLING THE PLUNGER TO DETER VANDALISM THAT IS CAST AS PART OF THE HOUSING COVER. USE A PLUNGER THAT PROTRUDES BEYOND THE SHROUD A DISTANCE ADEQUATE TO ACCOMMODATE THE SWITCH TRAVEL.

VERIFY THE LOCATION OF THE PUSH BUTTON ASSEMBLIES AND THE DIRECTION OF THE ARROWS ON THE SIGNS PRIOR TO INSTALLATION.

ALL NEW PEDESTRIAN SIGNAL HEADS TO BE COUNTDOWN.

PER TOWN OF ADDISON, PROVIDE POLARA IN2 APS PUSH BUTTONS AND CONTROLLER UNIT.

ITEM 6002, VEHICLE DETECTION:

PER TOWN OF ADDISON, PROVIDE AUTOSCOPE VISION VIVDS CAMERA AND PROCESSING SYSTEM.

ADDENDUM #1

6/15/2022



Kimley»Horn

13455 Noel Road
Two Galleries Office Tower, Suite 700
Dallas, Texas 75240

F-928
Tel. No. (972) 770-1300
Fax No. (972) 239-3820



**TRAFFIC SIGNAL
GENERAL NOTES**

SHEET 3 OF 3

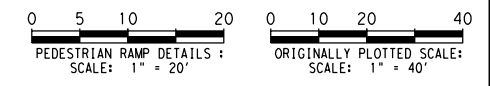
KHA PROJECT NUMBER: 063543039

SCALE: AS SHOWN

DATE: 6/15/2022

DESIGN TWH	GRAPHICS LMR	CHECK TWH
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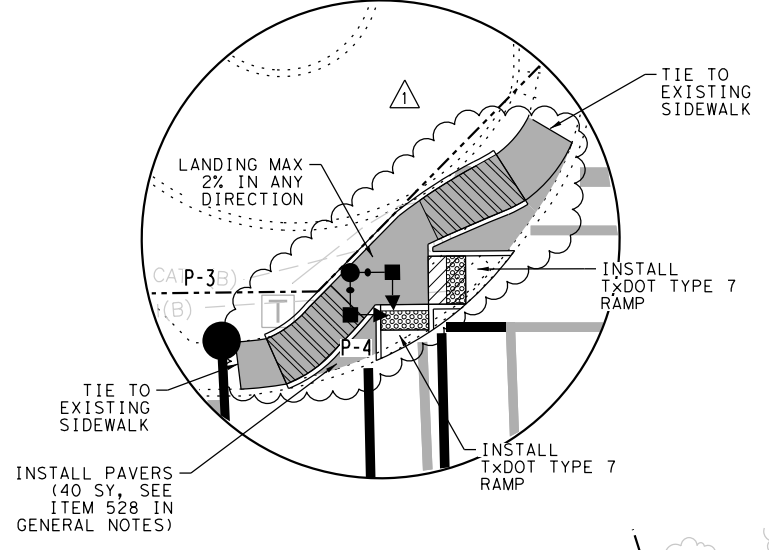
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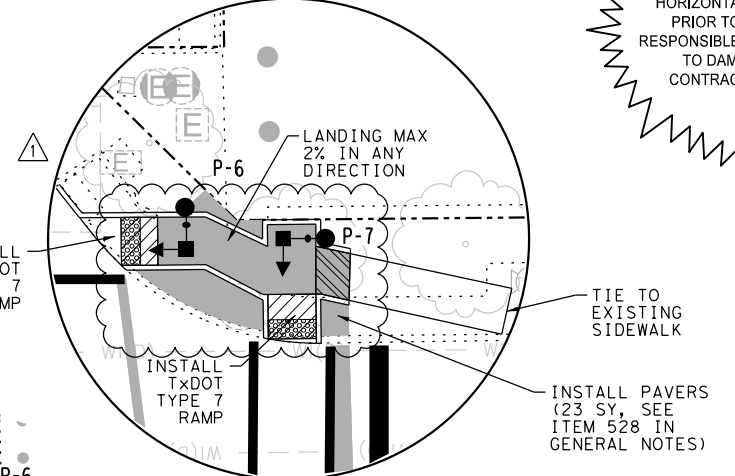
STOP!
CALL BEFORE YOU DIG
 DIG TESS
 1-800-DIG-TESS
 (@ least 72 hours prior to digging)

CAUTION!!
 EXISTING UNDERGROUND UTILITIES IN THE AREA
 CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE
 HORIZONTAL AND VERTICAL LOCATION OF ALL UTILITIES
 PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE
 RESPONSIBLE FOR ANY REPAIRS TO EXISTING UTILITIES DUE
 TO DAMAGE INCURRED DURING CONSTRUCTION.
 CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY
 DISCREPANCIES ON THE PLANS.

DETAIL AT NW CORNER



DETAIL AT NE CORNER



ADDENDUM #1

LEGEND

PAVEMENT MARKING

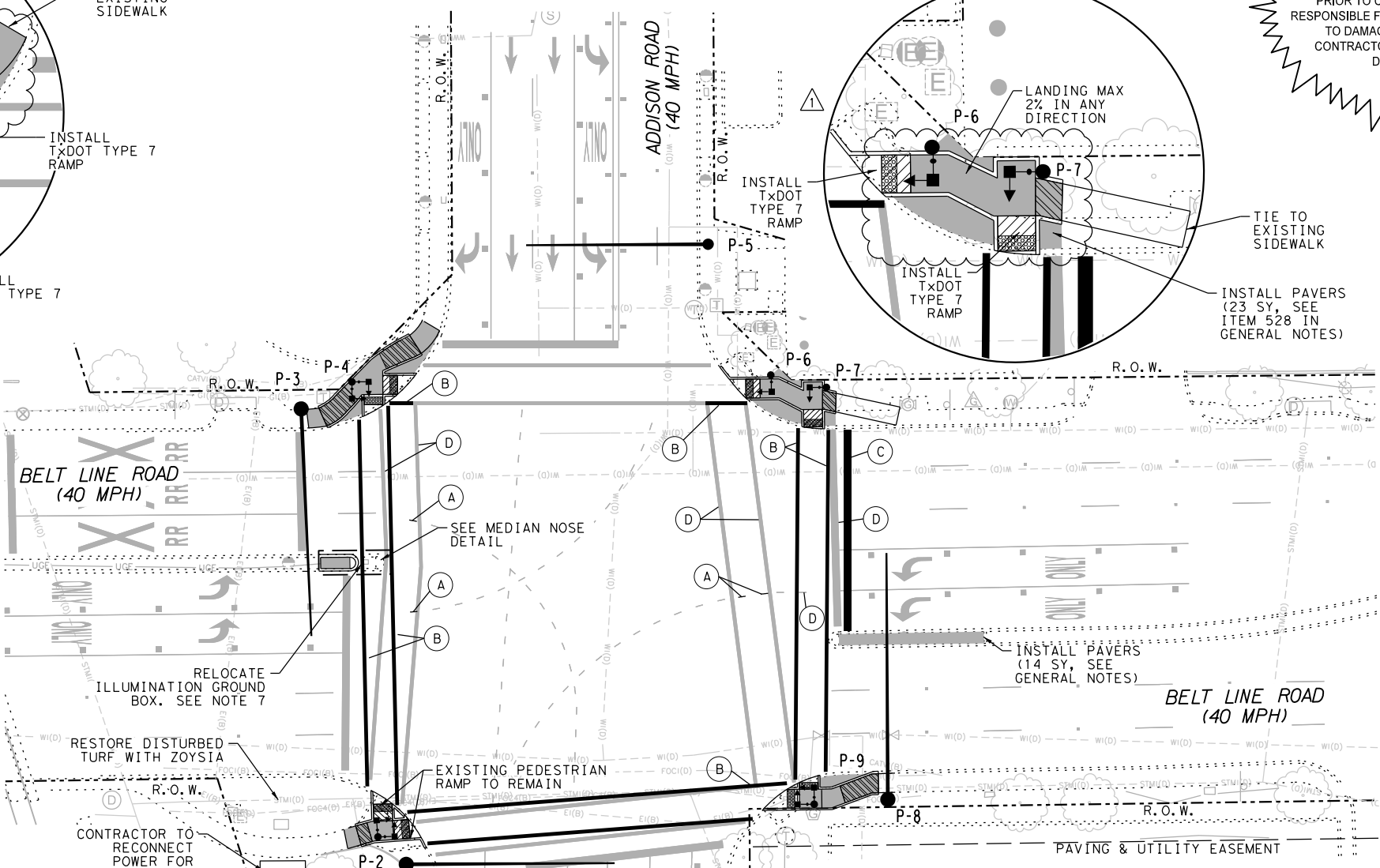
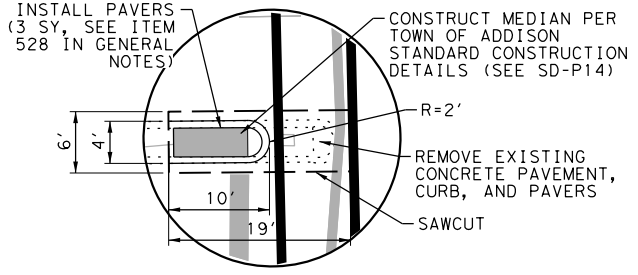
- (A) RE PM W/RET REQ TY I (W) 4" (BRK) (090MIL) (PUPPY TRACKS)
- (B) REFL PAV MRK TY I (W) 12" (SLD) (090MIL)
- (C) REFL PAV MRK TY I (W) 24" (SLD) (090MIL)
- (D) REMOVAL OF PAVEMENT MARKING LINE

LEGEND

PEDESTRIAN RAMPS

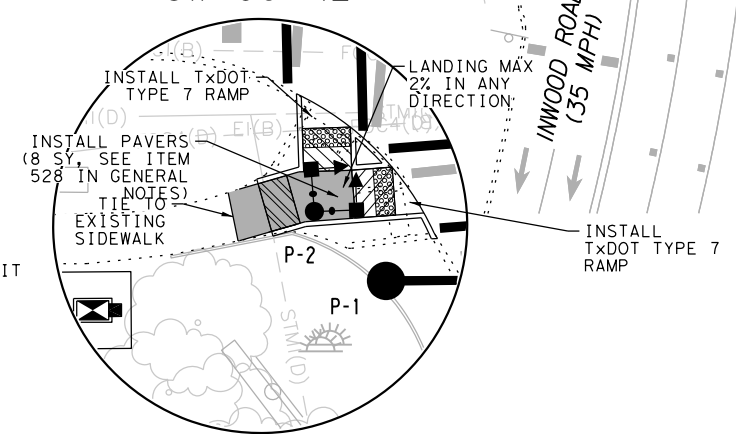
- 8.3% MAX RUNNING SLOPE
2% MAX CROSS SLOPE
- 5% MAX RUNNING SLOPE
2% MAX CROSS SLOPE
- TRUNCATED DOME
DETECTABLE WARNING
- BRICK PAVERS

MEDIAN NOSE DETAIL

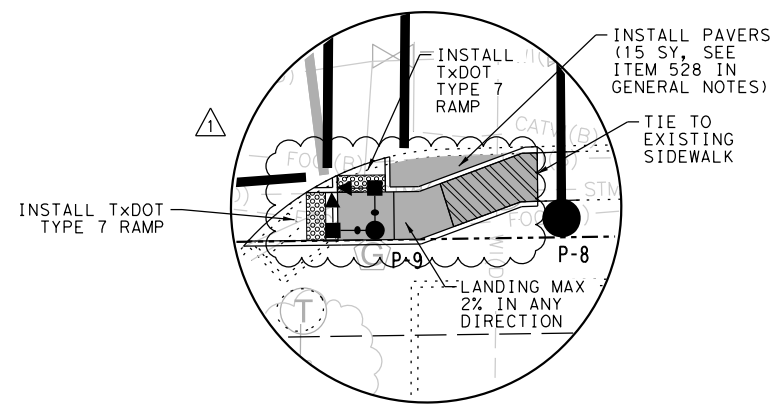


- NOTES:**
- INSTALLATION AND PAYMENT FOR PROPOSED RAMPS AND SIDEWALKS SHALL INCLUDE ALL INCIDENTAL WORK, INCLUDING EXCAVATION, REMOVAL AND DISPOSAL OF EXISTING CONCRETE CURB AND SIDEWALK, PROPOSED CURB ALONG SIDEWALKS, AND OTHER MISCELLANEOUS MATERIAL NECESSARY TO CONSTRUCT THE PROPOSED RAMPS AND SIDEWALKS. SIDEWALK QUANTITIES PROVIDED ARE ESTIMATES ONLY. PAYMENT FOR SIDEWALK SHALL BE FOR THE QUANTITY APPROVED BY THE ENGINEER AND CONSTRUCTED ON SITE.
 - PROPOSED CURB RAMP LANDING SHALL BE POURED UP TO THE SIGNAL FOUNDATION, LEAVING NO GAPS.
 - CONTRACTOR SHALL ENSURE THAT ALL PED RAMPS DRAIN TOWARDS THE STREET WITHOUT PONDING IN THE RAMP OR GUTTER AREA.
 - RAMP LANDINGS SHALL NOT EXCEED 2% MAX CROSS SLOPE AND 2% RUNNING SLOPE. PROPOSED SIDEWALKS SHALL NOT EXCEED 2% MAX CROSS SLOPE AND 5% RUNNING SLOPE.
 - EXISTING STRIPING AND BUTTONS SHALL REMAIN UNLESS OTHERWISE NOTED.
 - PEDESTRIAN ACCESS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.
 - CONTRACTOR TO MAINTAIN EXISTING ILLUMINATION CIRCUIT ALONG BELT LINE.

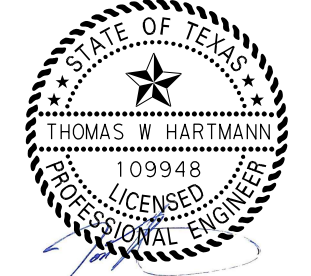
DETAIL AT SW CORNER



DETAIL AT SE CORNER



6/15/2022



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 Fax No. (972) 239-3820



**TRAFFIC SIGNAL PLANS
 PROPOSED PEDESTRIAN RAMPS
 AND PAVEMENT MARKINGS**

**BELT LINE ROAD AT
 ADDISON ROAD**

KHA PROJECT NUMBER: 063543039

SCALE: AS SHOWN

DATE: 6/15/2022

DESIGN TWH	GRAPHICS LMR	CHECK TWH
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PLOTTED: 6/15/2022
 FILENAME: K:\DAL_TPTO\project\063543039 - Addison_Signal_Construction_Support\CADD\Belt Line_SHT_016_Addison_Rd_Proposed_Ramps.dgn
 BY: Lucy Cunningham
 Support\CADD\Belt Line_SHT_016_Addison_Rd_Proposed_Ramps.dgn

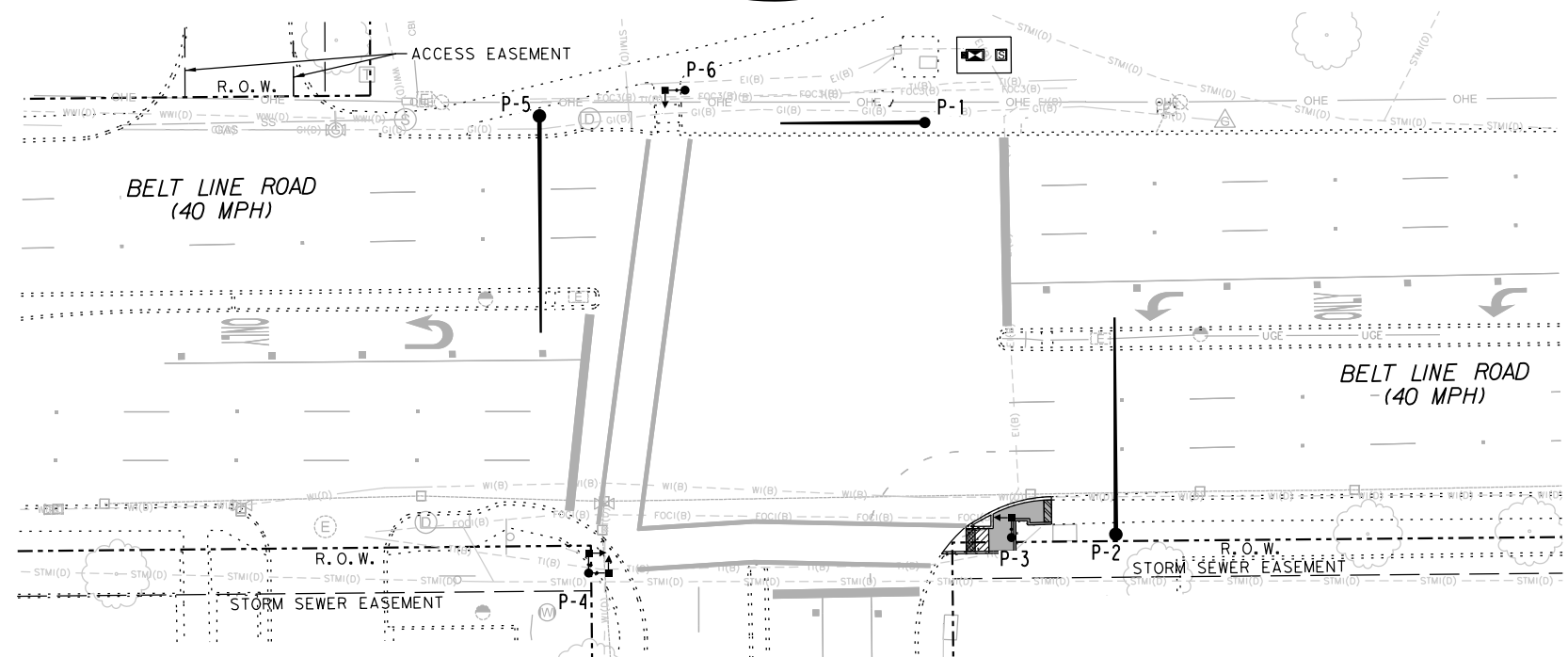
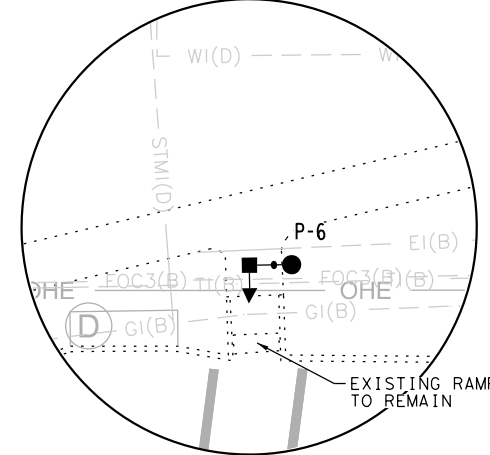


STOP!
CALL BEFORE YOU DIG
 DIG TESS
 1-800-DIG-TESS
 (@ least 72 hours prior to digging)

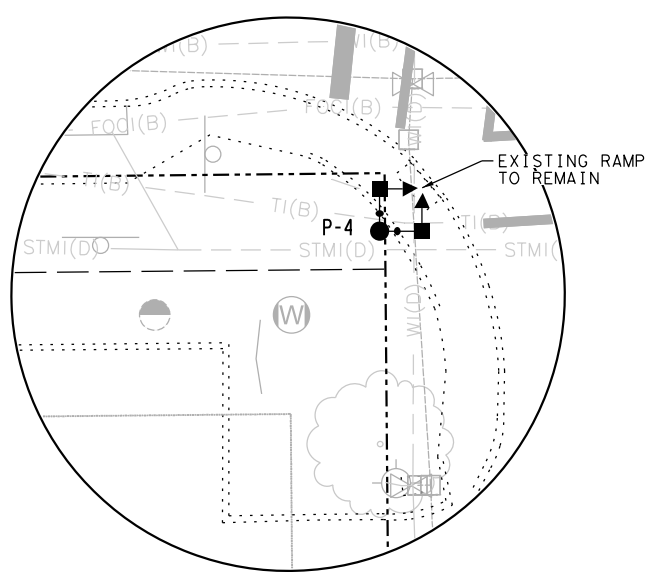
CAUTION!!
 EXISTING UNDERGROUND UTILITIES IN THE AREA
 CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE
 HORIZONTAL AND VERTICAL LOCATION OF ALL UTILITIES
 PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE
 RESPONSIBLE FOR ANY REPAIRS TO EXISTING UTILITIES DUE
 TO DAMAGE INCURRED DURING CONSTRUCTION.
 CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY
 DISCREPANCIES ON THE PLANS.

- NOTES:
1. INSTALLATION AND PAYMENT FOR PROPOSED SIDEWALKS SHALL INCLUDE ALL INCIDENTAL WORK, INCLUDING EXCAVATION, REMOVAL AND DISPOSAL OF EXISTING CONCRETE CURB AND SIDEWALK, PROPOSED CURB ALONG SIDEWALKS, AND OTHER MISCELLANEOUS MATERIAL NECESSARY TO CONSTRUCT THE PROPOSED SIDEWALKS. QUANTITIES PROVIDED ARE ESTIMATES ONLY. PAVEMENT FOR SIDEWALK SHALL BE FOR THE QUANTITY APPROVED BY THE ENGINEER AND CONSTRUCTED ON SITE.
 2. PROPOSED SIDEWALKS SHALL NOT EXCEED 2% MAX CROSS SLOPE AND 5% RUNNING SLOPE.
 3. EXISTING STRIPING AND BUTTONS SHALL REMAIN UNLESS OTHERWISE NOTED.
 4. PEDESTRIAN ACCESS SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION.

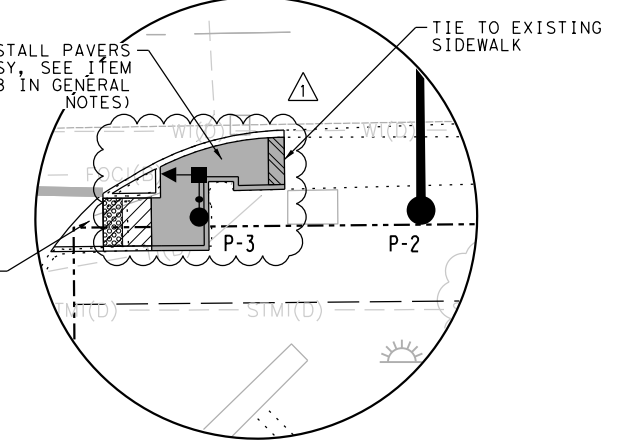
DETAIL AT NW CORNER



DETAIL AT SW CORNER



DETAIL AT SE CORNER

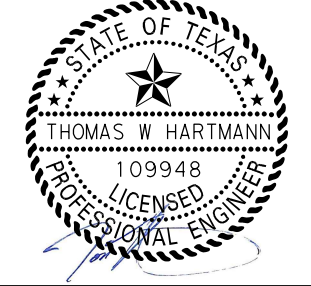


ADDENDUM #1

LEGEND
PEDESTRIAN RAMPS

	8.3% MAX RUNNING SLOPE 2% MAX CROSS SLOPE
	5% MAX RUNNING SLOPE 2% MAX CROSS SLOPE
	TRUNCATED DOME DETECTABLE WARNING
	BRICK PAVERS

6/15/2022



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TRAFFIC SIGNAL PLANS
 PROPOSED PEDESTRIAN RAMPS
 AND PAVEMENT MARKINGS

BELT LINE ROAD AT
 BELTWAY DRIVE

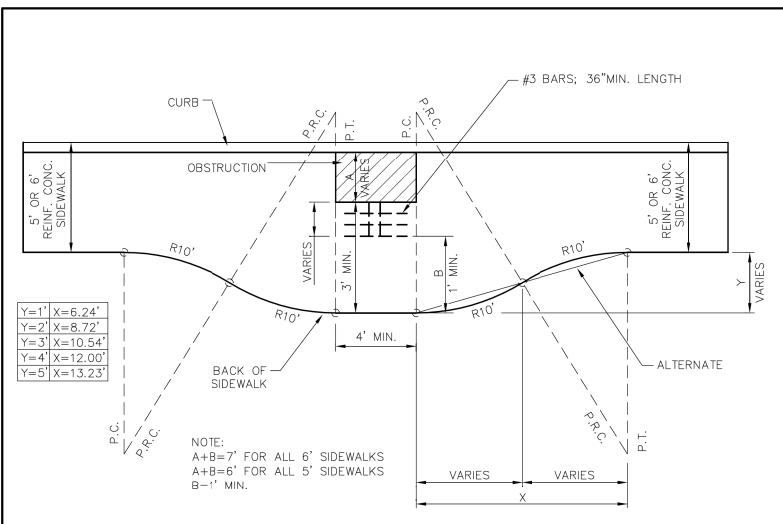
KHA PROJECT NUMBER: 063543039
 SCALE: AS SHOWN

DATE: 6/15/2022

DESIGN TWH	GRAPHICS LMR	CHECK TWH
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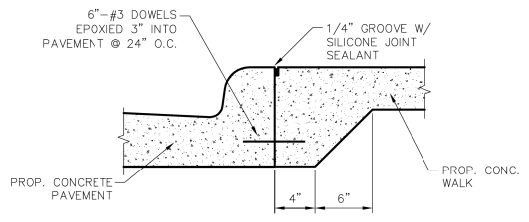
PLOTTED: 6/15/2022
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 BY: Lucy Cunningham
 \$\$\$SCALE\$\$\$
 \$\$\$PROJECT\$\$\$

6/15/2022 K:\DAL\TPTD\1\proj\063543039 - Addison Signal Construction Support\CADD\Standard\Standard.dwg Town of Addison\Addison-Standard.dwg
 PLOTTED BY: Lucy.Cunningham
 FILENAME:



SIDEWALK WIDENING REQUIRED AROUND OBSTACLE IN SIDEWALK

NTS



JOINT DETAIL FOR SIDEWALKS ADJACENT TO CURB

NTS

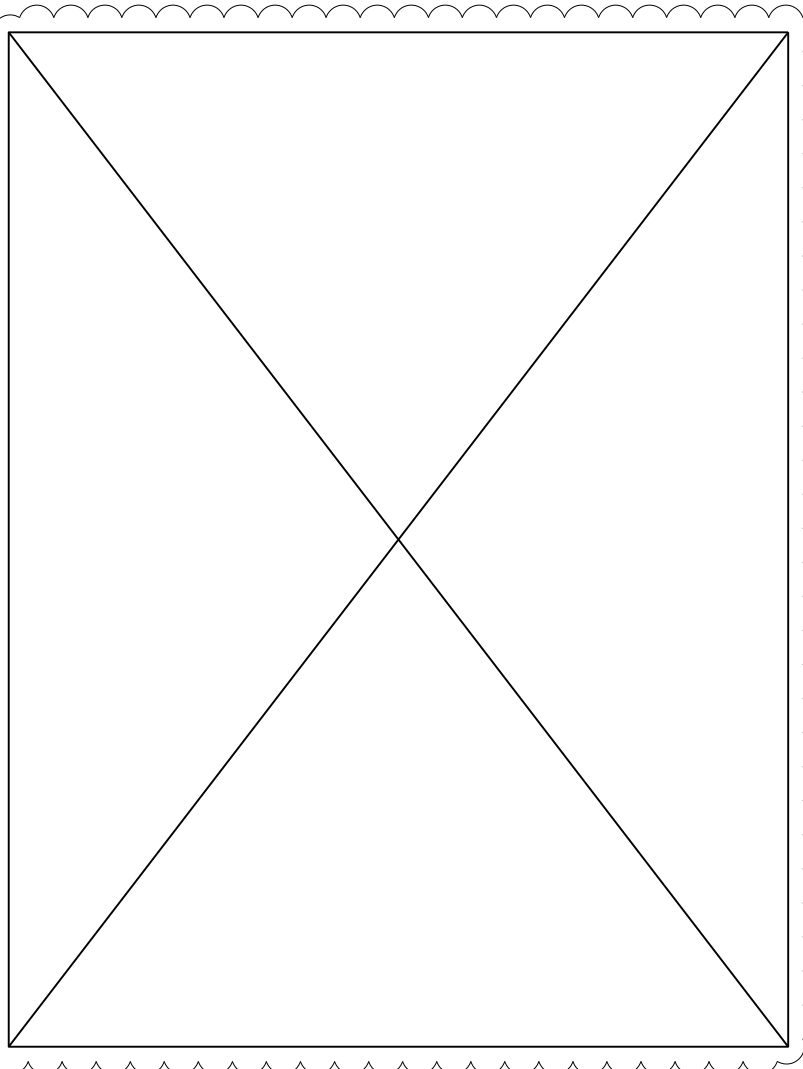


PUBLIC WORKS DEPARTMENT

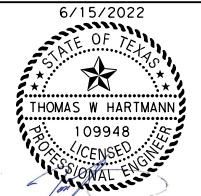
SIDEWALK WIDENING REQUIRED AROUND OBSTACLE IN SIDEWALK & JOINT DETAIL FOR SIDEWALKS ADJACENT TO CURB

STANDARD CONSTRUCTION DETAILS PAVING

DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-P26
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ADDENDUM #1



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TOWN OF ADDISON STANDARDS

SHEET 2 OF 6
KHA PROJECT NUMBER: 063543039

SCALE: AS SHOWN

DATE:

DESIGN	GRAPHICS	CHECK
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43

PLOTED: 6/15/2022
 FILENAME: K:\DAL\TPTD\1proj\rect\063543039 - Additon Signel Construction\Signel Construction\Standard\...Town of Addison\Additon Standards\ped.dgn
 BY: Lucy.Curnin@horns.com
 \$\$\$SCALE\$\$\$

GENERAL NOTES FOR PEDESTRIAN FACILITIES

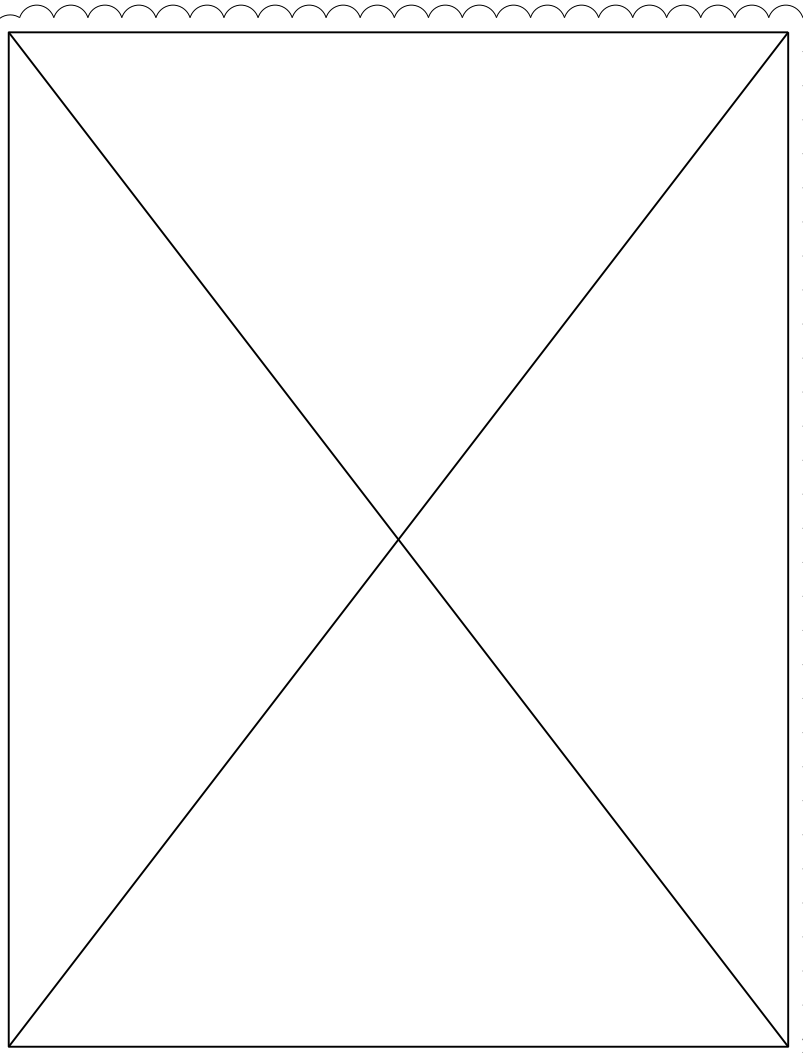
1. ALL SLOPES ARE MAXIMUM ALLOWABLE. THE LEAST POSSIBLE SLOPE THAT WILL STILL DRAIN PROPERLY SHOULD BE USED. ADJUST CURB RAMP LENGTH OR GRADE OF APPROACH SIDEWALKS AS DIRECTED.
2. LANDINGS SHALL BE 5'X5' MINIMUM WITH A MAXIMUM 2% SLOPE IN ANY DIRECTION.
3. MANEUVERING SPACE AT THE BOTTOM OF CURB RAMPS SHALL BE A MINIMUM OF 4'X4' WHOLLY CONTAINED WITHIN THE CROSSWALK AND WHOLLY OUTSIDE THE PARALLEL VEHICULAR TRAVEL PATH.
4. MAXIMUM ALLOWABLE CROSS SLOPE ON SIDEWALK AND CURB RAMP SURFACES IS 2%
5. CURB RAMPS WITH RETURNED CURBS MAY BE USED ONLY WHERE PEDESTRIANS WOULD NOT NORMALLY WALK ACROSS THE RAMP, EITHER BECAUSE THE ADJACENT SURFACE IS PLANTING OR OTHER NON-WALKING SURFACE OR BECAUSE THE SIDE APPROACH IS SUBSTANTIALLY OBSTRUCTED. OTHERWISE, PROVIDE FLARED SIDES.
6. ADDITIONAL INFORMATION ON CURB RAMP LOCATION, DESIGN, LIGHT REFLECTIVE VALUE AND TEXTURE MAY BE FOUND IN THE CURRENT EDITION OF THE TEXAS ACCESSIBILITY STANDARDS (TAS) AND 16 TAC §66.102.
7. CURB RAMPS SHALL BE ALIGNED WITH THEORETICAL CROSSWALKS, OR AS DIRECTED BY THE TOWN ENGINEER.
8. HANDRAILS ARE NOT REQUIRED ON CURB RAMPS. PROVIDE CURB RAMPS WHEREVER ON ACCESSIBLE ROUTE CROSSES (PENETRATES) A CURB.
9. FLARE SLOPE SHALL NOT EXCEED 10% MEASURED ALONG CURB LINE.
10. BARRIER FREE RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CURRENT EDITION OF THE TEXAS ACCESSIBILITY STANDARDS (TAS).
11. ALL BARRIER FREE RAMPS MUST PASS AN INDEPENDENT INSPECTION. A LETTER OF COMPLIANCE ACCEPTANCE IS REQUIRED PRIOR TO FINAL ACCEPTANCE BY THE TOWN OF ADDISON.
12. STREETS ON STEEP GRADE WILL REQUIRE LONGER TRANSITION ON UPGRADE SIDE.
13. MAXIMUM SLOPE ON RAMP PORTION SHALL NOT EXCEED 1" PER FOOT AT ANY LOCATION. VERTICAL DISTANCE BETWEEN STREET AND RAMP SHALL NOT EXCEED 3".

GENERAL NOTES FOR DETECTABLE WARNINGS

1. CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSIST OF RAISED TRUNCATED DOMES COMPLYING WITH SECTION 4.29 OF THE TEXAS ACCESSIBILITY STANDARDS (TAS). THE SURFACE MUST CONTRAST VISUALLY WITH THE ADJOINING SURFACES, INCLUDING SIDE FLARES. FURNISH DARK RED COLORED DETECTABLE WARNING SURFACE ADJACENT TO UNCOLORED CONCRETE AND CREAM COLORED DETECTABLE WARNING SURFACE ADJACENT TO DARK RED COLORED BRICK PAVERS.
2. DETECTABLE WARNING SURFACES MUST BE SLIP RESISTANT AND NOT ALLOW WATER TO ACCUMULATE.
3. ALIGN TRUNCATED DOMES IN THE DIRECTION OF PEDESTRIAN TRAVEL WHEN ENTERING THE STREET.
4. DETECTABLE WARNING SURFACES SHALL BE A MINIMUM OF 24" IN DEPTH IN THE DIRECTION OF PEDESTRIAN TRAVEL, AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR LANDING WHERE THE PEDESTRIAN ACCESS ROUTE ENTERS THE STREET.
5. DETECTABLE WARNING SURFACES SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS A MINIMUM OF 6" AND A MAXIMUM OF 8" FROM THE EXTENSION OF THE FACE OF CURB AND SHALL BE AN INTEGRAL PART OF THE WALKING SURFACE. DETECTABLE WARNING SURFACES MAY BE CURVED ALONG THE CORNER RADIUS.

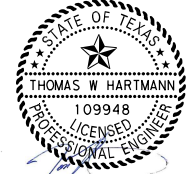
GENERAL NOTES FOR DETECTABLE WARNING PAVER UNITS

1. DETECTABLE WARNING PAVER UNITS SHALL MEET OR EXCEED ALL REQUIREMENTS OF ASTM C-936, C-33, AND BE LAID IN A TWO BY TWO UNIT BASKET WEAVE PATTERN OR AS DIRECTED.
2. LAY FULL-SIZE UNITS FIRST FOLLOWED BY CLOSURE UNITS CONSISTING OF AT LEAST 25 PERCENT OF A FULL UNIT. CUT DETECTABLE WARNING PAVER UNITS USING A POWER SAW.



ADDENDUM #1

6/15/2022



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TOWN OF ADDISON STANDARDS

SHEET 4 OF 6

KHA PROJECT NUMBER: 063543039

SCALE: AS SHOWN

DATE:

DESIGN GRAPHICS CHECK

45

Addison!

PUBLIC WORKS DEPARTMENT

PEDESTRIAN FACILITIES
GENERAL NOTES

STANDARD CONSTRUCTION DETAILS
PAVING

DATE: AUGUST, 2010	REV DATE: -	SHEET: SD-P37
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